

**STIPULATION OF PARTIES**  
**TO OPEN WILLIAM BURGESS BOULEVARD HIGHWAY-RAIL GRADE CROSSING**  
**AND CLOSE HARTS ROAD HIGHWAY-RAIL GRADE CROSSING**  
**NASSAU COUNTY, FLORIDA**

Nassau County, Florida (COUNTY), CSX Transportation, Inc., (RAILROAD), and the Florida Department of Transportation (DEPARTMENT), agree to this Stipulation of Parties as follows:

1. The COUNTY has filed applications with the DEPARTMENT to open William Burgess Boulevard highway-rail grade crossing and close the public highway-rail grade crossing at Harts Road, Crossing Number 620823 E, pursuant to Section 335.141(1), Florida Statutes. Copies of the applications are attached as EXHIBIT "A."
2. The William Burgess Boulevard crossing, approximately located at RAILROAD Milepost S 613.40, will be a two lane minor arterial with one sidewalk located on the southern side of the road. The Harts Road crossing is a two lane rural railroad crossing located at RAILROAD Mile Post S 614.11, in Nassau County, Florida (maps attached as EXHIBIT "B").
3. The railroad track, at William Burgess Boulevard and Harts Road locations, is a main line with seven train movements per day at a maximum speed of forty miles per hour.
4. The RAILROAD, at the COUNTY's expense, will provide all necessary materials and construct a railroad grade crossing at the extension of William Burgess Boulevard, in accordance with the DEPARTMENT's Standard Index No. 560, attached as EXHIBIT "C."
5. The RAILROAD, at the COUNTY's expense, will provide all necessary materials and install at William Burgess Boulevard highway-rail grade crossing Type III - Class III automatic railroad grade crossing warning devices (flashing lights and gates), in accordance with the DEPARTMENT's Standard Index Number 17882, attached as EXHIBIT "D."
6. The COUNTY, at its expense, will install W10-1 Advance Warning signs and pavement markings, in accordance with the the DEPARTMENT's Standard Index Number 17882 (see EXHIBIT "D").

7. The RAILROAD, at the COUNTY's expense, will maintain the William Burgess Boulevard crossing as needed and maintain the automatic railroad grade crossing warning devices annually. The COUNTY, at its expense, will maintain the roadway up to the ends of the railroad crossties, including pavement structure, pavement surface, shoulders, drainage, sidewalk, pavement striping, advance pavement markings, erosion control, tree cutting, mowing, and advance warning signs. Any work, by the COUNTY or RAILROAD, impacting the crossing and transportation operations will be coordinated between the COUNTY and RAILROAD, with an advance notice of no less than two weeks and a maintenance of traffic plan provided by the COUNTY.
8. The Harts Road highway-rail grade crossing closure will not occur until the William Burgess Boulevard highway-rail grade crossing is open to traffic. The RAILROAD will provide the COUNTY a minimum of forty-eight (48) hours notification, prior to the start of any closure work at the Harts Road crossing. This notification is to allow the COUNTY an opportunity to notify the public of the closure of Harts Road at the crossing location.
9. Prior to any work being performed in the removal of the Harts Road highway-rail grade crossing and signalization, the RAILROAD, at the COUNTY's expense, will erect on each side of the crossing, signs and object markers as identified in the DEPARTMENT's Standard Index 17349, attached as EXHIBIT "E." The COUNTY, at its expense, will be responsible for future maintenance of the signs and object markers indicating the closure of Harts Road.
10. The RAILROAD, at the COUNTY's expense, will remove the Harts Road crossing, roadway pavement within the RAILROAD's right-of-way; and the highway railroad grade crossing warning devices. The COUNTY, at its expense, will remove any advance warning signs and pavement markings pertaining to the Harts Road crossing. Through the removal of all evidence of the Harts Road crossing, the RAILROAD's right-of-way will be restored.
11. All work by the COUNTY and RAILROAD will be coordinated and performed in accordance with all applicable railroad requirements such as flagging, insurance, etc.
12. All parties agree that the closure of Harts Road crossing will occur within six months following the opening of the William Burgess Boulevard highway-rail grade crossing. The Harts Road and Clyatt Circle closures will offset the opening of the William Burgess Boulevard crossing (Stipulation of Parties for the Closure of Clyatt Circle attached as EXHIBIT "F").

13. All work by the COUNTY and RAILROAD will be consistent with the current Manual of Uniform Traffic Control Devices (MUTCD), American Association of State Highway and Transportation Officials (AASHTO) Policy, and the DEPARTMENT'S Manual of Uniform Minimum Standards for Design, Construction, and Maintenance for Streets and Highways (Florida's Green Book), and all COUNTY requirements.
14. The RAILROAD will assign a U.S. DOT crossing number to the William Burgess Boulevard rail crossing and complete the U.S. DOT Crossing Inventory Forms (OMB No. 2130-0017) for the opening of the new crossing at William Burgess Boulevard and the closure of the Harts Road crossing. The completed forms, as provided in EXHIBIT "G," will be submitted to the DEPARTMENT for inventory data entry and submittal to the Federal Railroad Administration.
15. This Stipulation of Parties has been executed by all parties having an interest in this matter. The RAILROAD and COUNTY waive administrative hearing rights provided by Chapter 120, Florida Statutes, for the opening and closure of the subject rail crossings.
16. The terms of this Stipulation of Parties may not be changed, waived, discharged or terminated orally, but only by an instrument or instruments in writing, signed by RAILROAD, COUNTY, and DEPARTMENT.
17. This Stipulation of Parties is governed by, and shall be interpreted, and construed in accordance with the laws of the State of Florida.
18. Any failure of any party to insist upon the strict performance of any terms or provisions of this Stipulation of Parties is not deemed to be a waiver of the terms of this agreement.
19. Under the terms set forth herein, the DEPARTMENT authorizes the opening of the William Burgess Boulevard highway-rail grade crossing and the closure of Harts Road highway-rail grade crossing as evidenced by the execution of this Stipulation of Parties.

(THIS CONCLUDES THE BODY OF THIS STIPULATION OF PARTIES)

CSX TRANSPORTATION, INC. - RAILROAD

By: Dale W. Ophardt  
(Authorized Signature)

Printed Name: Dale W. Ophardt  
Assistant Vice President Engineering

Title: \_\_\_\_\_

Date: October 9, 2009

NASSAU COUNTY - COUNTY

Owner:  
Board of County Commissioners  
Nassau County, Florida

[Signature]  
Chairman

Attest: [Signature]

MES  
8-27-09

Approved as to form by the County Attorney

[Signature]

STATE OF FLORIDA  
DEPARTMENT OF TRANSPORTATION

By: [Signature]  
State Public Transportation Administrator

Date: 10-16-09

DEPARTMENT OF TRANSPORTATION  
LEGAL REVIEW

By: JAM  
Attorney, FDOT

Date: 10-14-2009

EXHIBIT "A"

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION  
RAILROAD GRADE CROSSING APPLICATION

725-050-04  
RAIL  
10/06

ROAD NAME OR NUMBER	COUNTY/CITY NAME
William Burgess Boulevard Extension	Nassau County/Yulee

A. IDENTIFICATION:

Submitted By:

Application For:

Applicant Nassau County Brd of County Commissioners

Office Nassau County Engineering Services Dept

Telephone (904) 491-7330

Address 96161 Nassau Place, Yulee, FL 32097

- ☐ Closing a public at-grade rail/highway crossing by:
- ☐ roadway removal
  - ☐ rail line removal
- ☒ Opening a public at-grade rail/highway crossing by:
- ☐ new rail line construction
  - ☒ new roadway construction
- ☐ Conversion of an existing crossing by:
- ☐ private crossing to public at-grade rail/highway crossing

B. CROSSING LOCATION:

Jurisdiction for Street or Roadway by Authority of: ☐ City ☒ County ☐ State

FDOT/AAR Crossing Number: \_\_\_\_\_

Local Popular Name of Street or Roadway: William Burgess Boulevard Extension

Railroad Company: CSX Transportation, Inc.

Railroad Mile Post: 0613.46

Submitted for the Applicant by: Jonathan Page, P.E., Interim Director, Nassau County Eng Svcs DATE: 03/26/09  
Name and Title

Application FDOT Review by: Jan Bordelon, Rail Specialist DATE: 03/26/09  
Central Rail Office

REFERENCES:

(Specific Legal Authority) 334.044 F.S., 120.57 F.S.  
(Law Implemented) 335.141 F.S.  
(Administrative Rule) 14-46 003 F.A.C.  
Rail Manual Topic No: 725-050-002, Chapter 3

ATTACHMENTS SUBMITTED FOR ACCEPTANCE

- ☐ Crossing Closure Application Data
- ☐ Location Maps, Sketches, Plans (if available)
- ☐ Copies of Agreements Between Railroad and Jurisdiction on Subject Crossing
- ☐ Letters of Support (Opening or Closing)

## WILLIAM BURGESS BOULEVARD

### INFORMATION REQUEST: PUBLIC HIGHWAY-RAIL GRADE CROSSING (Provide current design plans, maps, aerials, and any support documentation)

1. Why is the crossing necessary?

*The crossing is necessary to provide an extension of William Burgess Boulevard to US 17. This road is vital for the community college and judicial complex in the area as well as a parallel evacuation corridor.*

2. Are there other access roads available to property owners if the crossing is not there?

*None are available.*

3. Has grade separation been considered in planning the crossing? If not, why?

*Yes, but it is too close to US 17 for a grade separated crossing.*

4. Vehicular transportation data:

a) Provide predicted Annual Average Daily Traffic (AADT) at the crossing.

*5,000 ADT's at opening.*

b) Provide level of service at the crossing.

*LOS D; two lanes – 13,600 daily vehicles, four lanes – 29,300 daily vehicles*

c) Provide anticipated AADT and level of service in 5 years.

*9,000 AADT LOS D for two lanes; LOS C for four lanes.*

d) Provide predicted percentage of truck traffic and anticipated truck traffic 5 years out.

*9% of truck traffic ; 810 daily vehicles.*

e) Will trucks carry hazardous materials? If so, approximately how many trips per day or week?

*No hazardous materials are anticipated along this governmental corridor.*

f) How many school buses will use the crossing?

*Approximately two (2) per day.*

g) How many emergency rescue vehicles will use the crossing?

*Approximately the same number that cross the tracks on Harts Road today (County does not have records of how many emergency vehicles use the crossing today)*

h) What is the vehicular design speed at the proposed crossing?

*35 mph*

i) How many thru or turn lanes?

*2 lanes for Phase I and 4 lanes for Phase II*

j) Will the road be divided with a median?

*Yes. (Phase II)*

- k) Will there be sidewalks and/or bike lanes?

*Sidewalks*

5. Rail transportation data:

- a) Current number and type of rail tracks.

*1 main track.*

- b) Current number of daily train movements.

*Seven (7) trains.*

- c) Approximate times during the day and evening that the crossing will be blocked.

*Mainly space throughout the morning and afternoon.*

- d) Approximate length of time (i.e. minutes) that the crossings are blocked.

*Depending on the length of the train, approximately 10 minutes (similar to Harris Road crossing that is proposed to be abandoned).*

- e) Speed of trains at crossing.

*3 to 40 mph.*

- f) Anticipated expansion of tracks and train movements.

*None.*

6. Provide excerpts from the Comprehensive Plan and any other transportation plans relative to the proposed crossing.

*Please find attached.*

7. What is the distance from the proposed crossing to adjacent public crossings? (Identify adjacent crossings by road name and crossing number.)

*2.0 miles*

8. What is the distance from the proposed crossing to the nearest intersection?

*1,100 feet to US 17 and William Burgess Boulevard (new intersection after crossing is constructed).*

9. Provide the following data on the crossing:

- Design Hourly Capacity (Design Year and Construction Year)

*2040 two lanes 1 500 per hour; 2020 four lanes 2 810 vehicles per hour.*

- Signal Timing/Turn Movements *Not Applicable*

- Trip Generation and Distribution. *Not Applicable*

- Intersection capacity analysis for railroad crossing intersecting roadway.

*Level of service is 3*

- Traffic operations and traffic safety issues evaluated for railroad crossing, train traffic movements, and railroad preemption.

*None.*

- Analysis should consider the traffic impacts of all build out proposed developments.

*The 5-year volume anticipates growth rates in the area.*

10. Provide description of land use on each side of the rail crossing.

*Industrial east and west of crossing.*

11. What is the predicted number of pedestrians and bike riders that will use the proposed crossing?  
What is the predicted number of users 5 years out?

*At opening- fewer than ten (10) daily for pedestrian and bike rides. After 5 years- approximately 20 daily. No new residential proposed in the immediate area.*

12. What is the degree of the angle of the road crossing the rail line?

*90 degrees.*

13. Are there any plans for any structures to be built near the crossing intersection?

*No.*

14. Identify major traffic generators (i.e., businesses, shopping malls, recreational areas, special events, etc.) in this area. Specify type, location, and distance to proposed crossing.

*One large proposed industrial development adjacent to the west. It only can generate 300 peak hour trips by permit. Other proposed projects are relatively small.*

15. Identify all highway traffic control devices, including pavement markings, signs, and highway traffic signals, that will be installed at the crossing. What future changes are proposed?

*Typical railroad crossing bars, striping, signage, signals and bells are proposed.*

16. How does the crossing impact the Annual Average Daily Traffic at nearby public crossings?  
Provide estimated traffic count changes, if any.

*The closing of the Harts Road crossing just south can easily be accommodated with this crossing. Some traffic will be removed from the AIA crossing as well. It is estimated that about 2,500 daily trips will be removed from AIA crossing.*

17. Please provide any corridor studies, or other preliminary traffic engineering studies that pertain to this crossing.

*Attached are engineering plans for the crossing.*

18. Please send maps, plans, safety studies and/or any other information that is relevant to the analysis of this crossing.

*An overall map showing the extension and proposed Harts Road closing is attached.*

19. Provide the names and crossing numbers of crossing closure candidates that would offset the opening of this crossing?

*These are already agreed upon in the attached stipulation of parties' agreement*



STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION  
RAILROAD GRADE CROSSING APPLICATION

725-060-06  
RAIL  
10%

ROAD NAME OR NUMBER

COUNTY/CITY NAME

Harts Road

Nassau County/Yulee

**A. IDENTIFICATION:**

Submitted By:

Application For:

Applicant Nassau County Board of County Comm

☒ Closing a public at-grade rail/highway crossing by:

☒ roadway removal

☐ rail line removal

Office Nassau County Engineering Services

☐ Opening a public at-grade rail/highway crossing by:

☐ new rail line construction

☐ new roadway construction

Telephone (904) 491-7330

☐ Conversion of an existing crossing by:  
☐ private crossing to public at-grade rail/highway crossing

Address 96161 Nassau Place, Yulee FL 32097

**B. CROSSING LOCATION:**

FDOT/AAR Crossing Number: 620823E

Jurisdiction for Street or Roadway by Authority of: ☐ City ☒ County ☐ State

Local Popular Name of Street or Roadway: Harts Road

Railroad Company: CSX Transportation, Inc.

Railroad Mile Post: CSX Mile Post S 0614.11

Submitted for the Applicant by: Jonathan Page, P.E., Interim Director, Nassau County Eng. Svcs DATE: 03/26/09

Application FDOT Review by: Jan Bordelon, Rail Specialist DATE: 03/26/09  
Central Rail Office

**REFERENCES:**

(Specific Legal Authority) 334.044 F.S., 120.57 F.S.

(Law Implemented) 335.141 F.S.

(Administrative Rule) 14-46.003 F.A.C.

Rail Manual Topic No: 725-060-002, Chapter 3

**ATTACHMENTS SUBMITTED FOR ACCEPTANCE:**

- ☒ Crossing Closure Application Data
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- ☐ Copies of Agreements Between Railroad and Jurisdiction on Subject Crossing
- ☐ Letters of Support (Opening or Closing)

## HARTS ROAD

### INFORMATION REQUEST: ANALYSIS FOR RAIL CROSSING CLOSURE

1. Why is the crossing a candidate for closure?

*The roadways have limited right of way, the crossing is not at 90 degrees and the new crossing can accommodate the displaced traffic.*

2. Are there other accessible means to the property if the crossing is closed?

*Yes, the new crossing can provide full access.*

3. Identify the alternate routes available if the crossing is closed?

*William Burgess Boulevard to Harts Road and A1A to Harts Road.*

4. What is the distance from the proposed crossing to adjacent public crossings? (Identify adjacent crossings by road name and crossing number.)

*Approximately 1 mile to the new William Burgess Boulevard crossing.*

5. What is the distance from the subject crossing to the nearest intersection? Identify the street.

*Approximately 1,000 feet to US 17 and Harts Road.*

6. How will the proposed crossing closure impact the Annual Average Daily Traffic at nearby public crossings? Provide estimated traffic count changes, if any.

*It is estimated that about five-hundred (500) ADT's will be displaced to the new crossing at William Burgess Boulevard.*

7. During peak time, calculate by driving the additional travel time and distance between two points (nearest intersection or major access) on either side of the subject crossing via alternate routes. Provide calculated times, routes, and distances.

*Time is less than 3 minutes; distance is less than 1 mile.*

8. What is the estimated annual daily number of vehicles, buses, trucks, pedestrians, and bike riders that currently use the subject crossing?

*Fewer than 550 daily combined.*

9. What is the estimated number of school buses and emergency vehicles that use the crossing?

*Approximately 2 per day. All emergency vehicles would use A1A to the north.*

10. How will transportation (vehicle, pedestrian, and cycle) be impacted if the crossing is closed? Consider capacity, level of service, congestion, and/or need for modification and additional traffic signals to the alternate roads where traffic will be routed when the crossing is closed.

*Traffic will easily use the new intersection at William Burgess Boulevard with US17 less than five (5) minutes and the crossing will be a safer condition. The level of service with the new crossing will be higher and no additional signal will be required.*

11. Is the proposed closure in conjunction with the County Comprehensive Plan or any other transportation plans?

*Yes and the stipulation of parties' agreement.*

12. How many train movements occur daily at the subject crossing?

*Seven*

13. Identify the number and type of trains traveling across the proposed crossing.

*Seven*

14. What is the minimum and maximum train speed at the crossing?

*5 mph to 40 mph*

15. Identify the number and type of tracks at the crossing.

*One (1) main track.*

16. What is the speed of the highway which crosses the rail line?

*25 mph. The new highway crossing speed limit will be 35 mph.*

17. Provide description of land use on both sides of the subject rail crossing.

*Residential to the west and east. Commercial to the east.*

18. Are there any churches, schools, or hospitals that will be impacted if the crossing is closed?  
Please list by name and location.

*None.*

19. Identify any major traffic generators (i.e., recreational areas, special events, etc.) in this area that will be impacted by the proposed crossing closure.

*None; all properties west of the tracks are large residential lots. Nearly all commercial traffic east of the project discharges to US 17.*

20. Identify all highway traffic control devices and highway traffic signals at nearby crossings that may require improvement or upgrading if the proposed crossing is closed.

*The new crossing will have standard new equipment.*

21. How will the proposed crossing closure impact the Annual Average Daily Traffic at nearby public crossings? Provide estimated traffic count changes, if any, at each identified crossing.

*The new crossing can accumulate the estimated 500 daily trips that will be displaced.*

22. Please send maps, plans, sketches and any other information that you feel is relevant to this project.

*Attached.*

[illegible]

**RESEARCH CONCLUSIONS**

ORDINANCE 2008 - 18

AN ORDINANCE OF THE BOARD OF COUNTY COMMISSIONERS OF NASSAU COUNTY FLORIDA AMENDING THE CAPITAL IMPROVEMENTS ELEMENT OF THE NASSAU COUNTY COMPREHENSIVE PLAN; ADOPTING THE FISCAL YEAR 2009-2013 SCHEDULE OF CAPITAL IMPROVEMENTS; PROVIDING FOR THE ANNUAL UPDATE OF THE SCHEDULE OF CAPITAL IMPROVEMENTS PURSUANT TO SECTION 163.3177(3)(b), FLORIDA STATUTES; PROVIDING FOR FINDINGS; PROVIDING FOR SEVERABILITY; AND PROVIDING AN EFFECTIVE DATE.

WHEREAS, on November 28, 2007 Nassau County adopted Ordinance 2007-38 amending the Capital Improvements Element of the Nassau County Comprehensive Plan and adopting a Schedule of Capital Improvements for FY 07/08 through 11/12; and

WHEREAS, Section 163.3177(3)(b), F.S. requires, by December 1<sup>st</sup> of every calendar year, each unit of local government to adopt by ordinance an annual update to the Schedule of Capital Improvements in order to maintain a financially feasible 5-year schedule of capital improvements incorporating those capital improvements necessary to maintain levels of service adopted as part of the Comprehensive Plan; and

WHEREAS, Section 8.5 of the Amended Interlocal Agreement and Sec. 163.3180(13), F.S. require the County to adopt by reference the Five-Year District Work Program adopted by the Nassau County School Board on September 23, 2008; and

WHEREAS, on September 30, 2008 Nassau County adopted a Capital Improvements Plan for FY 08/09 through 12/13 including improvements addressing level of service compliance and other improvements to expand capacity, correct existing deficiencies or meet policies of the Comprehensive Plan; and

WHEREAS, the Nassau County Planning And Zoning Board, also in their capacity as Local Planning Agency, held a duly noticed public hearing to address the requested amendment on November 4, 2008 and recommended unanimously approval; and

WHEREAS, pursuant to Chapters 125 and 163, F.S. public hearings have been held with due public notice having been provided.

NOW, THEREFORE, BE IT ORDAINED BY THE BOARD OF COUNTY COMMISSIONERS OF NASSAU COUNTY, FLORIDA:

*John F. ...*  
*County Commissioner*

#### SECTION 1. FINDINGS

This action complies with Chapter 163, Part II, Florida Statutes and Rule 9J-5, F.A.C. and is consistent with the goals, objectives and policies of the Nassau County Comprehensive Plan, in particular Policies 4.02.01, 9.01.02, 9.01.05, 9.05.05, 9.07.04 and 10.05.04.

#### SECTION 2. ANNUAL UPDATES

1. The FY '08/'09 through '12/'13 *Five-Year Schedule of Capital Improvements* to the Capital Improvements Element, is hereby adopted as set forth in Exhibit "A", attached hereto and made a part hereof.
2. The FY '08/'09 through '12/'13 *Revenue Projections/Capital Funding Sources* to the Capital Improvements Element, is hereby adopted as set forth in Exhibit "B", attached hereto and made a part hereof.
3. The *Nassau County School District's Five-Year Work Plan* for FY '08/'09 through '12/'13 as set forth in Exhibit "C", attached hereto and made a part hereof, is hereby adopted by reference into the Capital Improvements Element.

#### SECTION 4. SEVERABILITY

It is the intent of the Board of County Commissioners of Nassau County, Florida, and is hereby provided, that if any section, subsection, sentence, clause, phrase, or provision of this Ordinance is held to be invalid or unconstitutional by any court of competent jurisdiction, such invalidity or unconstitutionality shall not be so construed as to render invalid or unconstitutional the remaining provisions of this Ordinance.

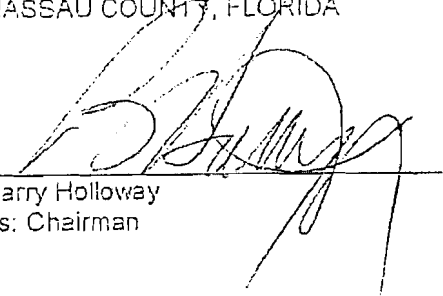
#### SECTION 3. EFFECTIVE DATE

This Ordinance shall be filed with the Office of the Secretary of State. This Ordinance shall become effective upon the earlier of:

- i. The Department of Community Affairs issues a final order determining the adopted amendment is found to be in compliance; or
- ii. The Administration Commission issues a final order determining the adopted amendment to be in compliance.

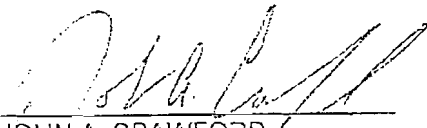
Adopted this 21<sup>st</sup> day of November, 2008 by the Board of  
County Commissioners of Nassau County, Florida.

BOARD OF COUNTY COMMISSIONERS  
NASSAU COUNTY, FLORIDA



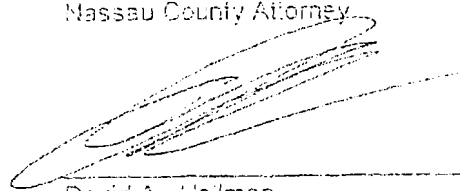
Barry Holloway  
Its: Chairman

ATTEST as to Chairman's Signature:



JOHN A. CRAWFORD  
Its: Ex-Officio Clerk

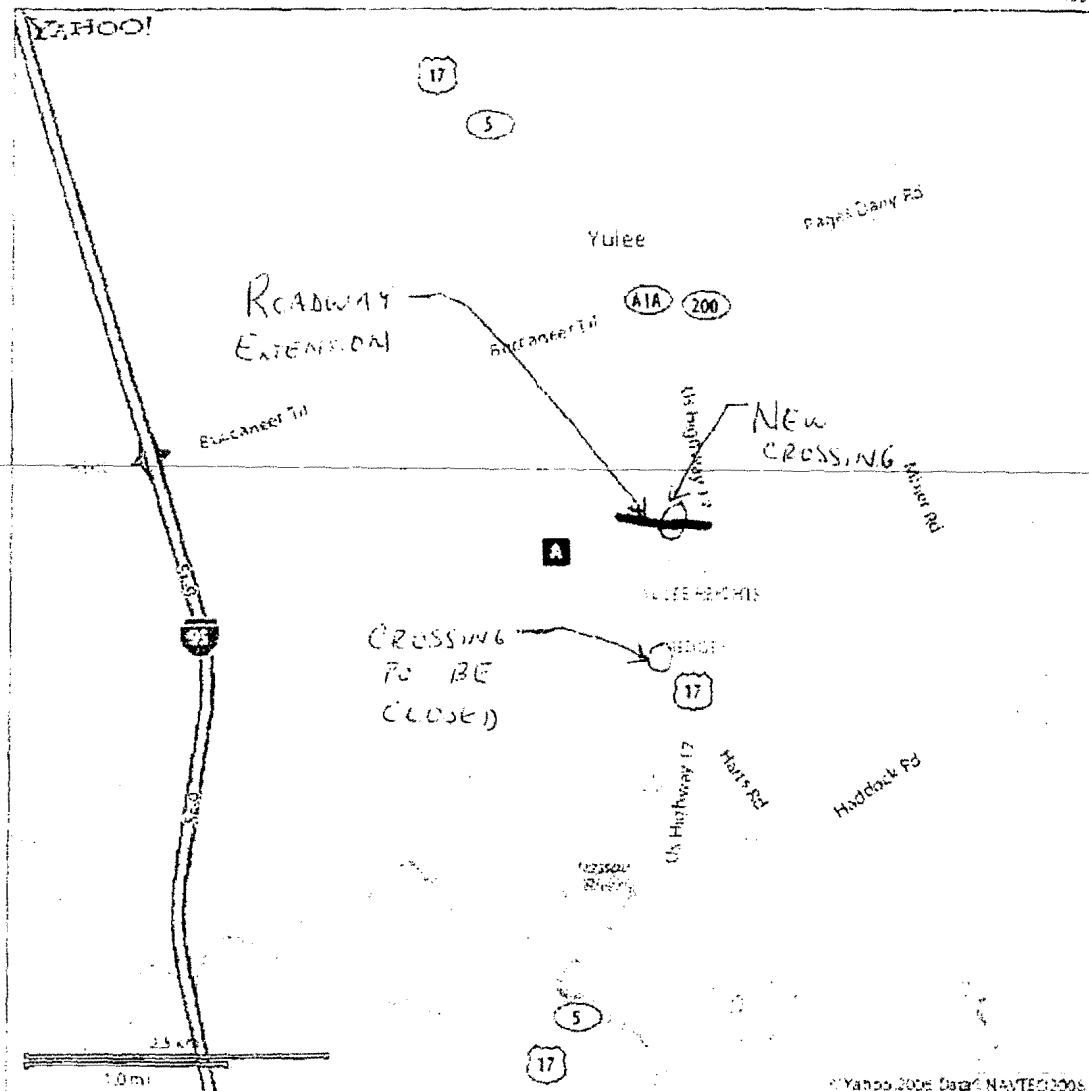
Approved as to form by the  
Nassau County Attorney.



David A. Hallman,  
County Attorney

Map of 30.611784,-81.611768

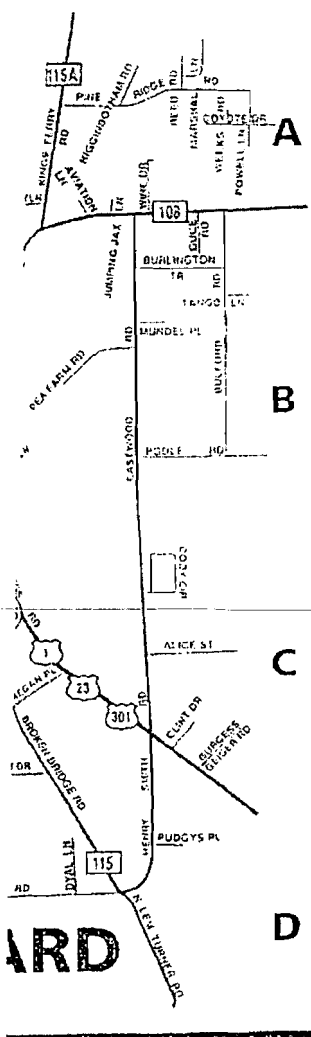
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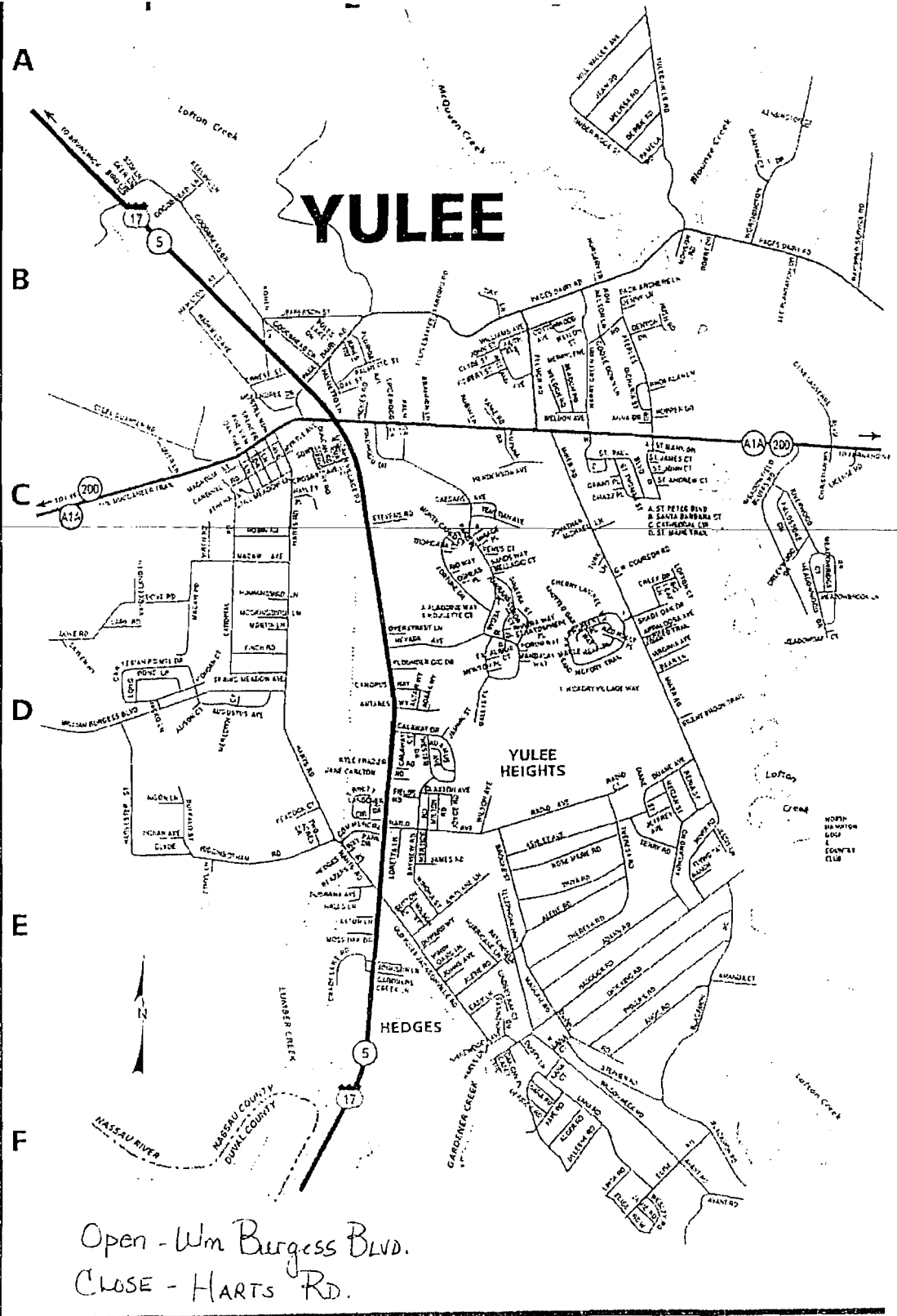
When using any driving directions or map, it's a good idea to do a double check and make sure the road still exists. Watch out for construction, etc. Take all the safety precautions. This is only to be used as a guide in planning.



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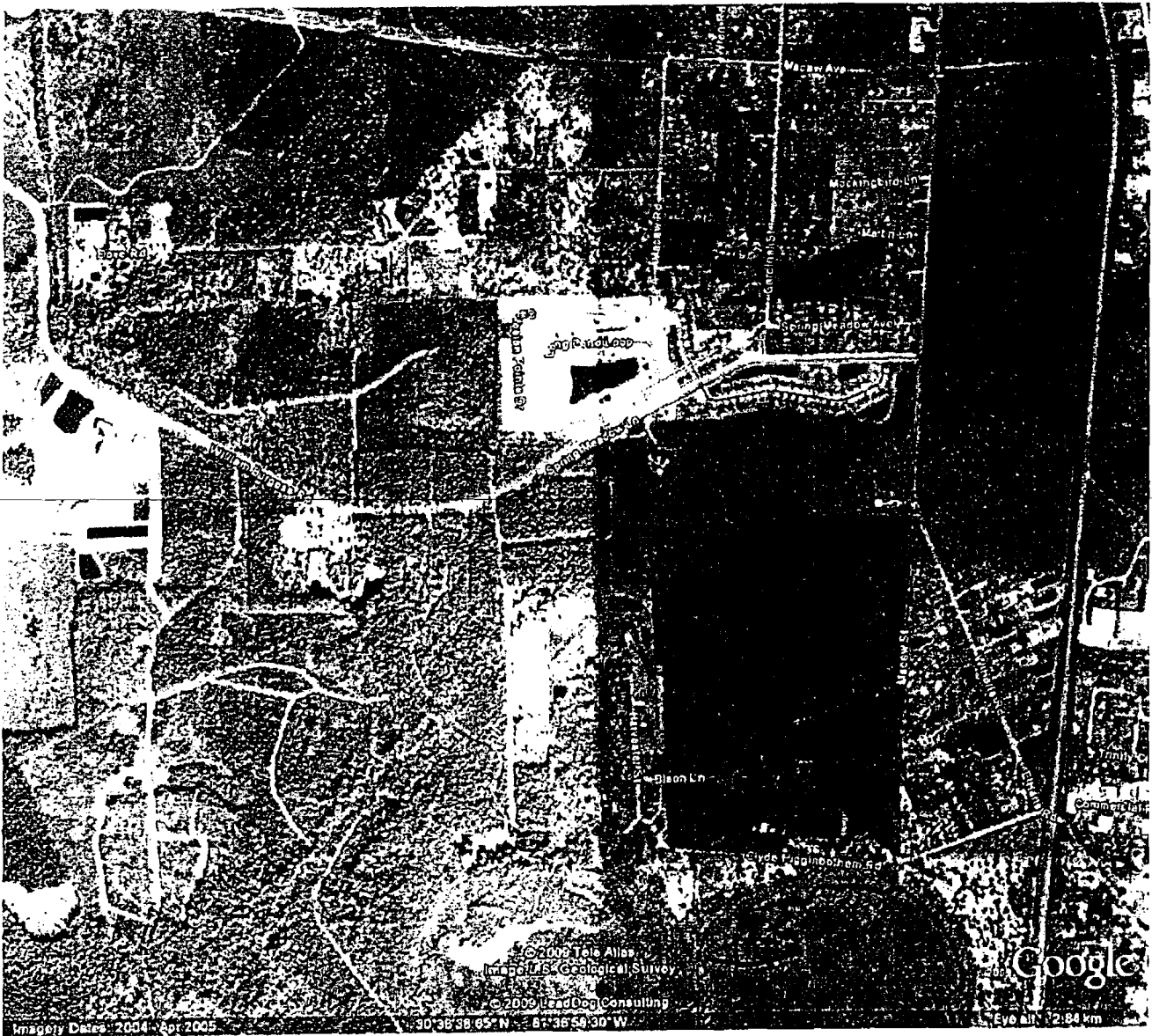
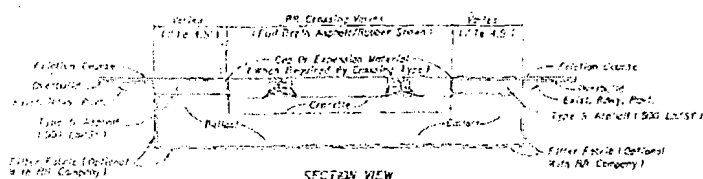
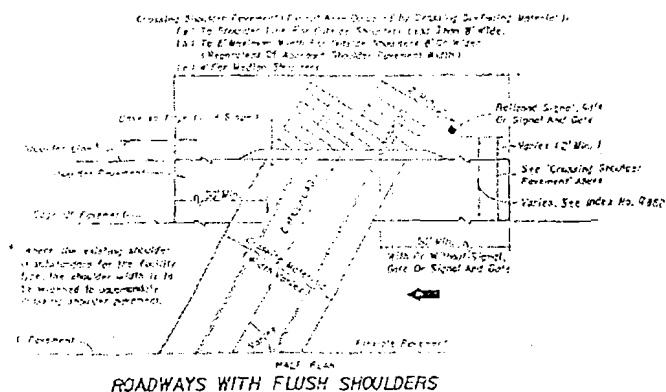


Image Date: 2004 Apr 2005

30°38'38.65"N 87°36'58.30"W

Eye Alt: 12.84 km

EXHIBIT "C"

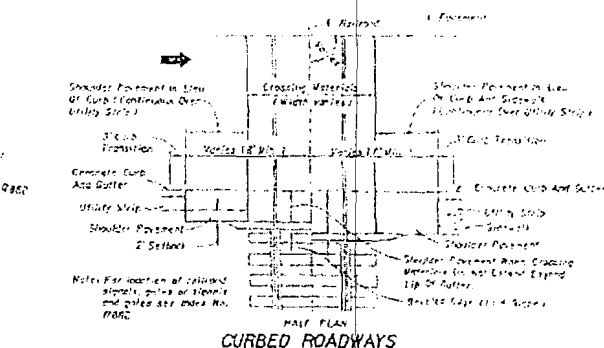


TYPICAL FLEXIBLE PAVEMENT REPLACEMENT AT PANEL RR CROSSINGS

CHOOSING SURFACES	
Type	Description
1	Concrete
2	Asphalt
3	Gravel
4	Grass
5	Soil
6	Water
7	Ice
8	Snow
9	Other

STOP ZONE FOR RUDDER CROSSING	
Boat Speed (mph)	Zone Length (Distance From Mast)
45 or less	250'
40 - 45	300'
35 - 40	500'
30	600'

- Type 2: Drawings are used for multiple first prints or other uses for an existing or proposed building or site. Drawings are created once.
- In the first Type 2 drawings within the scope of the plan may be combined engineering or safety considerations where appropriate.



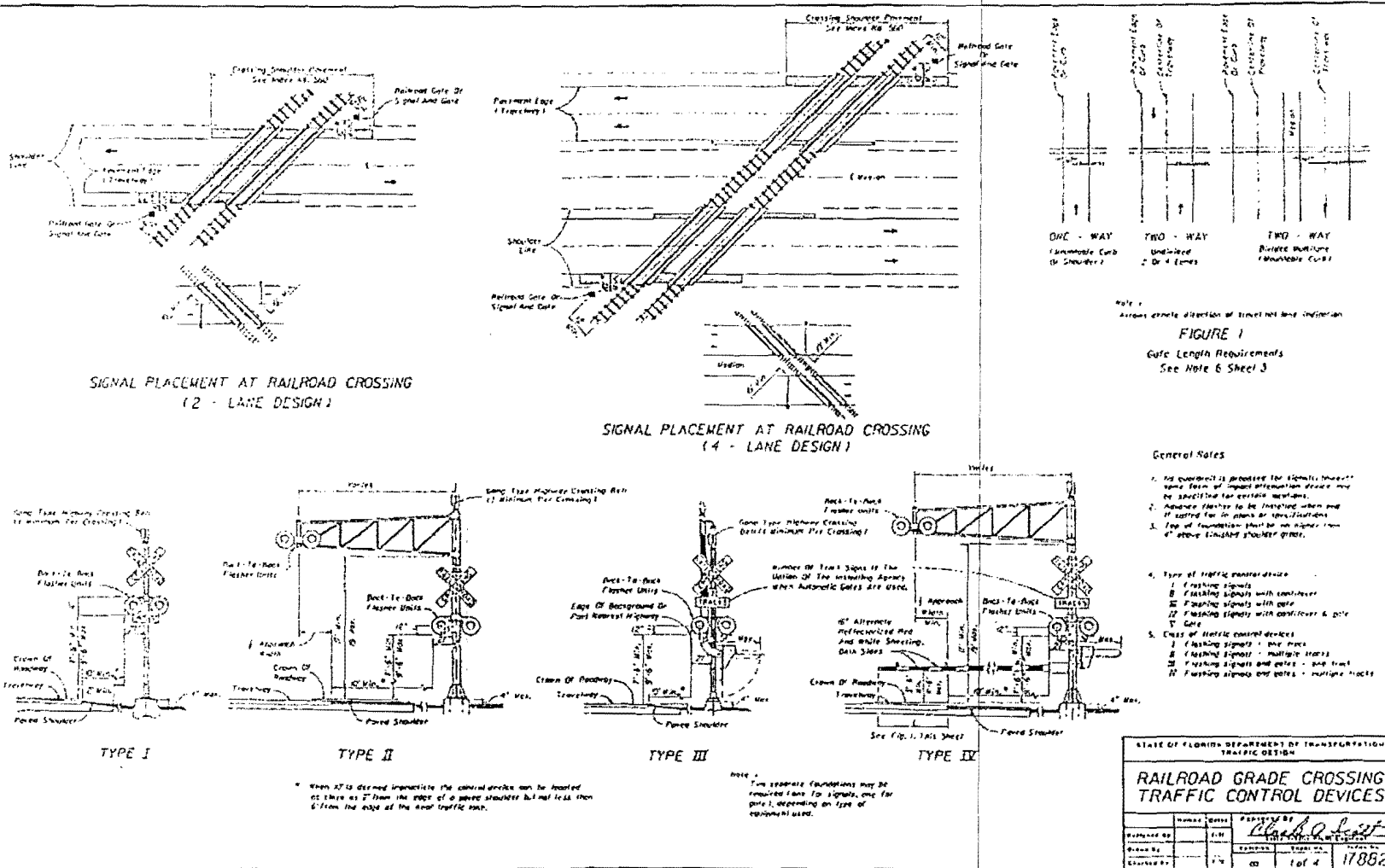
To prevent low-voltage arcing from occurring on the tracks, the crossing surface should be at the same plane as the top of the rails for a distance of 2 feet out from the rails. The surface should be made of concrete or steel plate 1 inch higher at the ends than the top of the nearest rail point to the track ends (see final substation notes at different level arcing). Vertical curves should be used to transition from the  $\frac{1}{2}$ -inch plate to a level point at the beginning of the rails. Rails that are not insulated, or a roadway adjacent surface that is not insul., will necessitate a fire specific analysis for rail clearances.

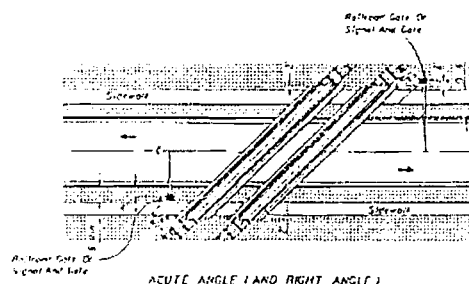
### VERTICAL ROADWAY ALIGNMENT THROUGH A RAILROAD CROSSING

### General Notes

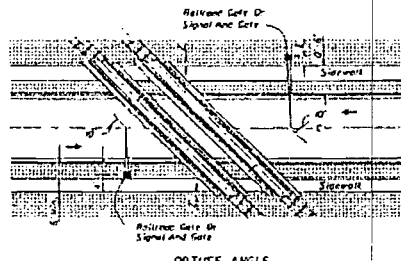
1. The Bidder/Contractor will furnish and install all Road Bed, Subgrade, drainage, utility, existing surface, easels and necessary materials. All materials, equipment, including that through the crossing, will be furnished and installed by the Department or its contractor, unless registered otherwise.
2. When a replacement short crossing is involved within the limits of a highway construction project, a transition pavement will be constructed at the expense of the crossing to remove substantially impacts to the crossing. The transition pavement will be maintained as appropriate to protect the crossing from low clearance vehicles and vehicular impacts until the construction project is completed and the final highway surface is constructed.
3. The District/State Police will maintain a list of currently used Railroad Crossing Products and will periodically distribute the current list to the District Offices as the list is updated.
4. The Bidder/Contractor shall submit engineering drawings for the proposed crossing advance type to the Construction Project Engineer and to the District Office for review and concurrence along with the list of Railroad Crossing Products. The approved engineering drawings of the crossing advance type shall be made a part of the Substitution agreement.
5. Sightways shall be constructed through the crossing between adjacent segments of the crossing. Sightways shall be constructed with appropriate material to allow unobstructed travel through the crossing in accordance with ADA requirements.
6. All access ways to the easement in accordance with Index No. 504 and Section 303 of the Standard Specifications.

[illegible]





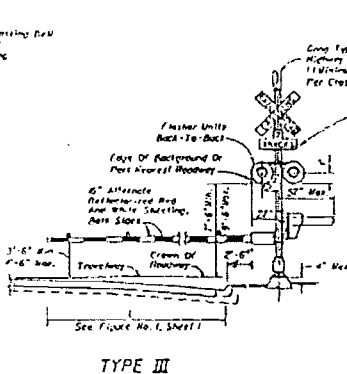
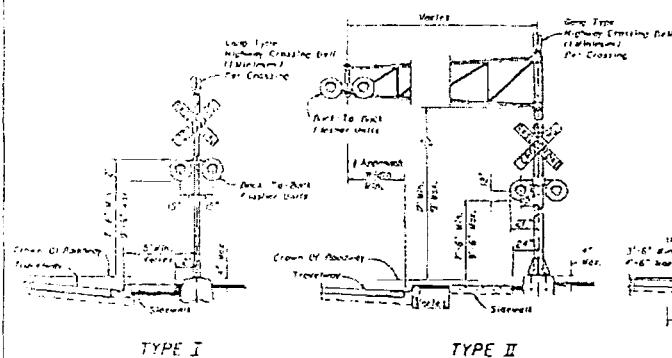
SIGNAL PLACEMENT AT RAILROAD CROSSING  
(2 LANES, CURB & GUTTER)



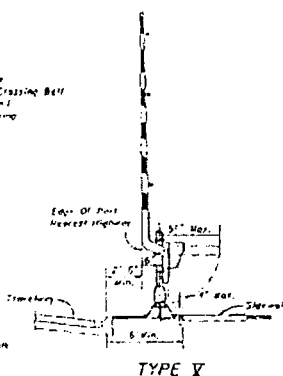
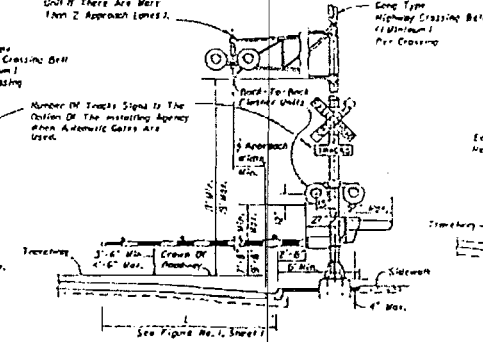
SIGNAL PLACEMENT AT RAILROAD CROSSING  
(2 LANES, CURB & GUTTER)

#### GENERAL NOTES

1. The location of flashing signals and stop lines shall be established based on future (or present) inspection of gullies with appropriate road clearance.
2. Where plans call for railroad traffic control devices to be installed in curved sections, the minimum median width shall be 15' 0".
3. Location of railroad traffic control device is based on the distance methods between face of curb & sidewalk, 6' to 8' - (Leave device nearest sidewalk, Type B) - (Leave device between face of curb and sidewalk).
4. Stop line to be perpendicular to edge of roadway, corner 15' from nearest rail or 8' from and parallel to gate when present.

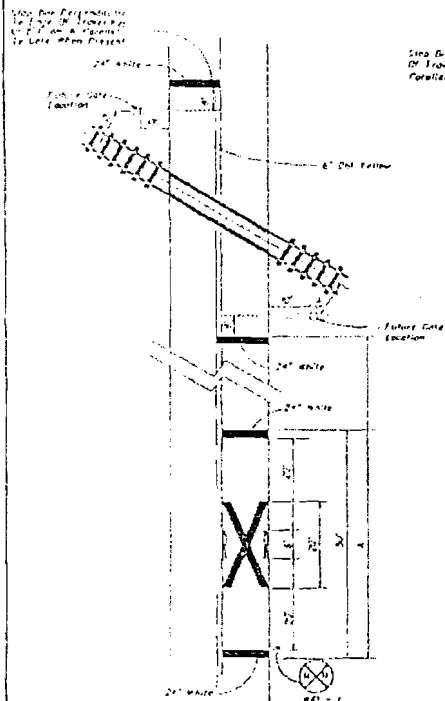


As A Minimum, Position One Flasher Unit Over Lane Separation Lines 150' From One Flasher Unit If There Are More Than 2 Approach Lanes.



STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION TRAFFIC DESIGN			
RAILROAD GRADE CROSSING TRAFFIC CONTROL DEVICES			
DESIGNED BY	DATE	REVISION	BY
DRW: M	1-11		
APPROVED BY	DATE	REVISION	BY
	1-11		
2 of 4		17882	

1945, four defendants, the  
 1st, 2nd, 3rd, 4th, 5th, 6th, 7th, 8th, 9th, 10th, 11th, 12th, 13th, 14th, 15th, 16th, 17th, 18th, 19th, 20th, 21st, 22nd, 23rd, 24th, 25th, 26th, 27th, 28th, 29th, 30th, 31st, 32nd, 33rd, 34th, 35th, 36th, 37th, 38th, 39th, 40th, 41st, 42nd, 43rd, 44th, 45th, 46th, 47th, 48th, 49th, 50th, 51st, 52nd, 53rd, 54th, 55th, 56th, 57th, 58th, 59th, 60th, 61st, 62nd, 63rd, 64th, 65th, 66th, 67th, 68th, 69th, 70th, 71st, 72nd, 73rd, 74th, 75th, 76th, 77th, 78th, 79th, 80th, 81st, 82nd, 83rd, 84th, 85th, 86th, 87th, 88th, 89th, 90th, 91st, 92nd, 93rd, 94th, 95th, 96th, 97th, 98th, 99th, 100th, 101st, 102nd, 103rd, 104th, 105th, 106th, 107th, 108th, 109th, 110th, 111th, 112th, 113th, 114th, 115th, 116th, 117th, 118th, 119th, 120th, 121st, 122nd, 123rd, 124th, 125th, 126th, 127th, 128th, 129th, 130th, 131st, 132nd, 133rd, 134th, 135th, 136th, 137th, 138th, 139th, 140th, 141st, 142nd, 143rd, 144th, 145th, 146th, 147th, 148th, 149th, 150th, 151st, 152nd, 153rd, 154th, 155th, 156th, 157th, 158th, 159th, 160th, 161st, 162nd, 163rd, 164th, 165th, 166th, 167th, 168th, 169th, 170th, 171st, 172nd, 173rd, 174th, 175th, 176th, 177th, 178th, 179th, 180th, 181st, 182nd, 183rd, 184th, 185th, 186th, 187th, 188th, 189th, 190th, 191st, 192nd, 193rd, 194th, 195th, 196th, 197th, 198th, 199th, 200th, 201st, 202nd, 203rd, 204th, 205th, 206th, 207th, 208th, 209th, 210th, 211th, 212th, 213th, 214th, 215th, 216th, 217th, 218th, 219th, 220th, 221st, 222nd, 223rd, 224th, 225th, 226th, 227th, 228th, 229th, 230th, 231st, 232nd, 233rd, 234th, 235th, 236th, 237th, 238th, 239th, 240th, 241st, 242nd, 243rd, 244th, 245th, 246th, 247th, 248th, 249th, 250th, 251st, 252nd, 253rd, 254th, 255th, 256th, 257th, 258th, 259th, 260th, 261st, 262nd, 263rd, 264th, 265th, 266th, 267th, 268th, 269th, 270th, 271st, 272nd, 273rd, 274th, 275th, 276th, 277th, 278th, 279th, 280th, 281st, 282nd, 283rd, 284th, 285th, 286th, 287th, 288th, 289th, 290th, 291st, 292nd, 293rd, 294th, 295th, 296th, 297th, 298th, 299th, 300th, 301st, 302nd, 303rd, 304th, 305th, 306th, 307th, 308th, 309th, 310th, 311th, 312th, 313th, 314th, 315th, 316th, 317th, 318th, 319th, 320th, 321st, 322nd, 323rd, 324th, 325th, 326th, 327th, 328th, 329th, 330th, 331st, 332nd, 333rd, 334th, 335th, 336th, 337th, 338th, 339th, 340th, 341st, 342nd, 343rd, 344th, 345th, 346th, 347th, 348th, 349th, 350th, 351st, 352nd, 353rd, 354th, 355th, 356th, 357th, 358th, 359th, 360th, 361st, 362nd, 363rd, 364th, 365th, 366th, 367th, 368th, 369th, 370th, 371st, 372nd, 373rd, 374th, 375th, 376th, 377th, 378th, 379th, 380th, 381st, 382nd, 383rd, 384th, 385th, 386th, 387th, 388th, 389th, 390th, 391st, 392nd, 393rd, 394th, 395th, 396th, 397th, 398th, 399th, 400th, 401st, 402nd, 403rd, 404th, 405th, 406th, 407th, 408th, 409th, 410th, 411th, 412th, 413th, 414th, 415th, 416th, 417th, 418th, 419th, 420th, 421st, 422nd, 423rd, 424th, 425th, 426th, 427th, 428th, 429th, 430th, 431st, 432nd, 433rd, 434th, 435th, 436th, 437th, 438th, 439th, 440th, 441st, 442nd, 443rd, 444th, 445th, 446th, 447th, 448th, 449th, 450th, 451st, 452nd, 453rd, 454th, 455th, 456th, 457th, 458th, 459th, 460th, 461st, 462nd, 463rd, 464th, 465th, 466th, 467th, 468th, 469th, 470th, 471st, 472nd, 473rd, 474th, 475th, 476th, 477th, 478th, 479th, 480th, 481st, 482nd, 483rd, 484th, 485th, 486th, 487th, 488th, 489th, 490th, 491st, 492nd, 493rd, 494th, 495th, 496th, 497th, 498th, 499th, 500th, 501st, 502nd, 503rd, 504th, 505th, 506th, 507th, 508th, 509th, 510th, 511th, 512th, 513th, 514th, 515th, 516th, 517th, 518th, 519th, 520th, 521st, 522nd, 523rd, 524th, 525th, 526th, 527th, 528th, 529th, 530th, 531st, 532nd, 533rd, 534th, 535th, 536th, 537th, 538th, 539th, 540th, 541st, 542nd, 543rd, 544th, 545th, 546th, 547th, 548th, 549th, 550th, 551st, 552nd, 553rd, 554th, 555th, 556th, 557th, 558th, 559th, 560th, 561st, 562nd, 563rd, 564th, 565th, 566th, 567th, 568th, 569th, 570th, 571st, 572nd, 573rd, 574th, 575th, 576th, 577th, 578th, 579th, 580th, 581st, 582nd, 583rd, 584th, 585th, 586th, 587th, 588th, 589th, 590th, 591st, 592nd, 593rd, 594th, 595th, 596th, 597th, 598th, 599th, 600th, 601st, 602nd, 603rd, 604th, 605th, 606th, 607th, 608th, 609th, 610th, 611th, 612th, 613th, 614th, 615th, 616th, 617th, 618th, 619th, 620th, 621st, 622nd, 623rd, 624th, 625th, 626th, 627th, 628th, 629th, 630th, 631st, 632nd, 633rd, 634th, 635th, 636th, 637th, 638th, 639th, 640th, 641st, 642nd, 643rd, 644th, 645th, 646th, 647th, 648th, 649th, 650th, 651st, 652nd, 653rd, 654th, 655th, 656th, 657th, 658th, 659th, 660th, 661st, 662nd, 663rd, 664th, 665th, 666th, 667th, 668th, 669th, 670th, 671st, 672nd, 673rd, 674th, 675th, 676th, 677th, 678th, 679th, 680th, 681st, 682nd, 683rd, 684th, 685th, 686th, 687th, 688th, 689th, 690th, 691st, 692nd, 693rd, 694th, 695th, 696th, 697th, 698th,



SPEED MPH	"A" IN FT
60	550
55	450
50	375
45	300
40	225
35	150
30	100
USUAL	50 MPH

Send Our Correspondence to Love  
Of Travel May Or 8 From A  
Familiar To Gals When Present.

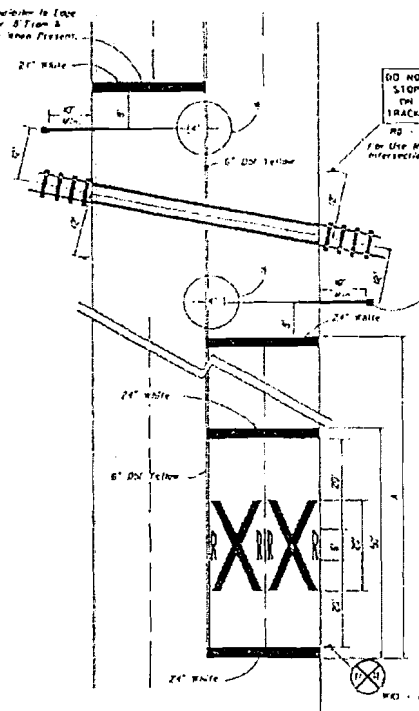
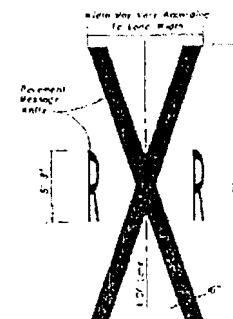


Diagram illustrating the location of a flashing signal with a gate. The diagram shows a cross-section of a road and a railway track. A 'Stop Line' is marked on the road. A 'Gate Or Flashing Signal With Gate' is located at the 'Edge Of Footway'. A 'Flashing Signal (If Not With Gate)' is also shown. The distance from the 'Stop Line' to the 'Gate' is labeled 'B''. The distance from the 'Edge Of Footway' to the 'Gate' is labeled 'A''. The distance from the 'Edge Of Footway' to the 'Flashing Signal' is labeled 'A''. The distance from the 'Edge Of Footway' to the 'Flashing Signal' is labeled 'As Required'.



1. When displaying pavement message, symbols are not include transverse lines.
2. Placement of sign W10-1 is recommended as business district, where low speeds are present, the W10-1 sign may be placed a minimum distance of 100 ft from the crossing. Where direct intersections occur between the RXX pavement message and the tracks an additional W10-1 sign and additional pavement message should be used.
3. Recommended location for sign F1P-38, RXX Urban & XOT three in advance of the crossing.
4. A portion of the pavement markings symbol should be directly opposite the W10-1 sign.
5. Recommended location for F1P-38 A or B signs, RXX urban and XOT rural. See memo #355 for sign details.

4. 6. Gate Length Requirements

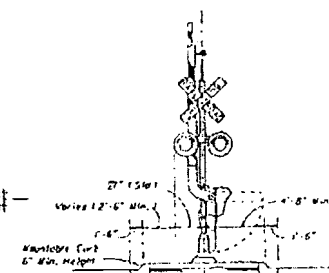
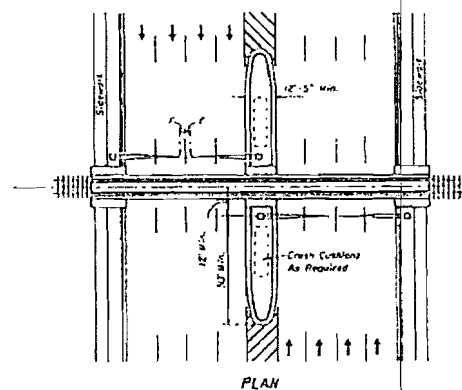
For two-way undivided sections:  
The gate should extend to within 1 ft of the center line. On multilane roadways the maximum gate length may not reach to within 1 ft of the center line, for these cases, the distance from the gate to the center line shall be a maximum of 4'.

For one-way or divided sections:  
The gate shall be of sufficient length such that the distance from the gate tip to the inside edge of pavement is a maximum of 4'.

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION TRAFFIC DESIGN			
RAILROAD GRADE CROSSING TRAFFIC CONTROL DEVICES			
Project No.	XXXXXX	Phase	Approval By <i>[Signature]</i>
Drawn By	XXXXXX	Scale	Sheet No. 1 of 1
Checked By	XXXXXX	Date	1788



Specification Length Of Gate Arm	Dimension "A"	Dimension "B"	Dimension "C"
4 FL	6"	36"	5'
15 FL	10"	36"	5'
6-10 FL	24"	36"	5'
6-9 FL	28"	41"	5'
20-23 FL	28"	4'	5'
19-20 FL	28"	5'	5'
20-30 FL	36"	6'	6'
32-34 FL	36"	7'	7'
35-37 FL	36"	8'	8'
38 And Over	36"	12'	12'



MEDIAN SECTION AT SIGNAL GATES

NOTE .

For additional information see the "Manual On Uniform Traffic Control Devices", Part VII; The "Traffic Control Devices Manual", Part VIII and AASHTO "A Policy On Graphic Design Of Streets And Highways".

MEDIAN SIGNAL GATES FOR  
MULTILANE UNDIVIDED URBAN SECTIONS  
(THREE OR MORE DRIVING LANES IN ONE DIRECTION, 45 mph OR LESS)

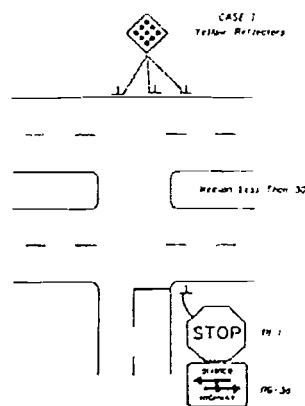
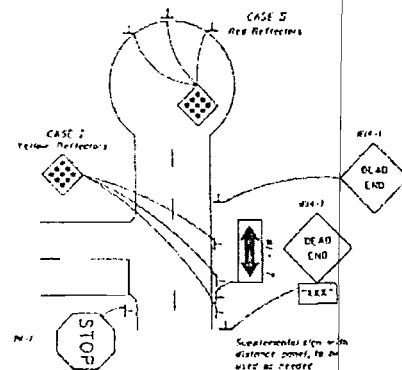
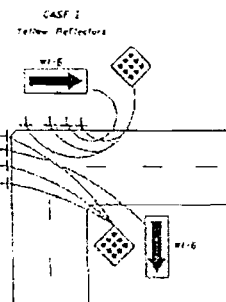
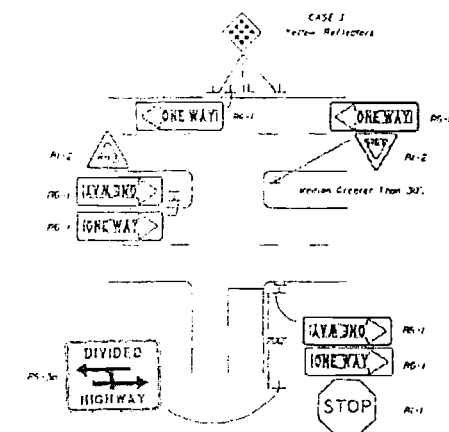
STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION TRAFFIC SIGNAL			
RAILROAD GRADE CROSSING TRAFFIC CONTROL DEVICES			
Worksheet No.	Sheet	Assigned By <i>Charles L. ...</i> State Traffic Signal Office	
Section No.	Sheet No.	Station	Sheet No.
Control No.		01	4 of 4 17882

**CASE 1** Type I object reflectors (R10-1) and 3" x 6" yellow reflective background or white reflective background on a yellow reflective background or white reflective background on a yellow reflective background.

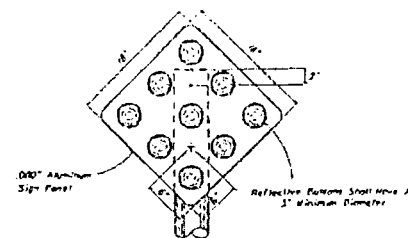
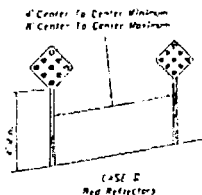
**CASE 2** Type II object reflectors (R10-2) and 3" x 6" yellow reflective background or white reflective background on a yellow reflective background or white reflective background on a yellow reflective background.

#### NOTES:

1. This note applies to residential street intersections only. Main streets to be indicated on a case by case basis.
2. "T" intersection two-way street and reflectors are optional. The note should be noted by a review of each location.
3. For additional object reflectors (R10-1) and 3" x 6" yellow reflective background or white reflective background on a yellow reflective background or white reflective background on a yellow reflective background.
4. Case 1 installation: The arrow pointing and object reflectors should be spaced approximately 20' but not less than 12' from the edge of the travel lane.
5. Dead end sign should be placed a sufficient distance to permit the vehicle operator to make the dead end by turning left, if possible, at the nearest intersecting street.
6. For pavement marking see index no. 1005.
7. No pavement is required unless specific traffic conditions require its use.



One way signs (R1-1) are not normally needed at divided highway intersections with median width of less than 30' and should be installed only if specifically called for in the plans.



Supports shall be set into the ground.

2" x 1/2" x 1/2" Aluminum Round Post or 2.5" x 1/2" Steel Flanged Channel Post.

Aluminum Posts: 1/2" x 1/2" Aluminum Round Post with 1/2" x 1/2" x 1/2" Stainless Steel Hex Head Bolt with Flat Washer under Head and Lockwasher under Nut.

Channel Posts: Provide attachment in accordance with the "Sign Attachment Details" on index no. 1005.

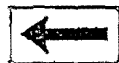
STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION					
TRAFFIC CONTROLS FOR STREET TERMINATIONS					
Project No.	Sheet No.	Revision No.	Scale	Date	17.3.49
Drawn By	Checked By	Reviewed By	Approved By	1 of 1	



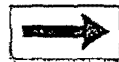
Diagram illustrating the layout of a single-lane highway with a centerline and approach lanes. The diagram shows the following components and dimensions:

- Centerline:** Labeled "CENTER OF ROADWAY".
- Approach Lanes:** Labeled "CENTER OF APPROACH LANE".
- Signs:**
  - Two diamond-shaped signs labeled "DM4-3" with dimensions "10' X 16'" and "10' X 10'".
  - A rectangular sign labeled "W1-CL" with dimensions "48' X 84'".
  - A rectangular sign labeled "DM4-3" with dimensions "10' X 10'".
- Dimensions:**
  - Distance between the two diamond signs: "4' MIN."
  - Distance between the diamond sign and the rectangular sign: "5' MIN."
  - Distance between the rectangular sign and the diamond sign: "2' MIN."
  - Distance between the diamond sign and the edge of the approach lane: "1'-9"
- Other Labels:**
  - "CENTER OF OPPOSITE LANE"
  - "LINE OF OPPOSITE LANE"
  - "TOP OF RAIL"
  - "EDGE OF PAVEMENT"
  - "FLARED END TREATMENT"
  - "EDGE OF APPROACH LANE"
  - "STD. W-BEAM GUARDRAIL (LENGTH VARIES) REFER TO STATE DESIGN STANDARDS"
  - "POST MOUNTS AND SPACING VARY REFER TO STATE STANDARDS"
  - "TYPE X STEEL POST (TYPE) SEE DETAIL FOR MOUNTING"

OPTIONAL WARNING NUCLON  
USE AS APPROPRIATE



(47-68)



(WT-62)



89-7

Diagram illustrating a closed road end configuration with various signs and dimensions:

- Signs:**
  - ROAD CLOSED:** A rectangular sign mounted on a post.
  - ROAD ENDS:** A rectangular sign mounted on a post.
  - W-4-3:** Diamond-shaped advance warning signs for the closed road end.
- Dimensions and Spacing:**
  - Sign Spacing:** 10' MIN. between the W-4-3 signs.
  - Sign Size:** 10' X 30' for the ROAD CLOSED sign, 16' X 10' for the ROAD ENDS sign, and 10' X 10' for the W-4-3 signs.
  - Post Height:** 6'-6" for the ROAD CLOSED sign, 4' MIN. for the ROAD ENDS sign, and 4' MIN. for the W-4-3 signs.
  - Sign Mounting:** 10' X STEEL POST (TYP.) SEE DETAIL FOR MOUNTING.
  - Sign Placement:** 10' OF RAIL, 2' MIN. from the edge of the opposite lane.
  - Sign Placement:** 1' - 5' from the edge of the approach lane.
- Standards:**
  - POST MOUNTS AND SPACING VARY REFER TO STATE STANDARDS**
  - STD. W-BEAM GUARDRAIL (LENGTH VARIES) REFER TO STATE DESIGN STANDARDS**
  - FLARED END TREATMENT REFER TO STATE DESIGN STANDARDS**
- Other Labels:**
  - E. OF REMOVED ROADWAY:** Indicated by an arrow pointing to the top of the diagram.
  - EDGE OF OPPOSITE LANE:** Indicated by a dashed line.
  - EDGE OF PAVEMENT:** Indicated by a solid line.
  - EDGE OF APPROACH LANE:** Indicated by a dashed line.

SOME STATES MAY REQUIRE  
THIS VERSION OF THE FBI-2

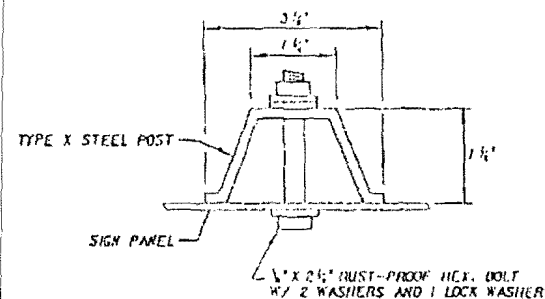
SOME STATES MAY REQUIRE  
THIS VERSION OF THE FBI-2

## NOTES

GUARDRAIL AND SIGN PANELS SHALL BE INSTALLED IN ACCORDANCE WITH FDOT ROAD AND SIGN STANDARDS, FDOT SPECIFICATIONS AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ADVANCE WARNING SIGNS, TURN PROHIBITION SIGNS, OR ANY OTHER NECESSARY TRAFFIC CONTROL DEVICE THAT IS NOT LOCATED ON CSX PROPERTY SHALL BE INSTALLED BY OTHERS.

### MOUNTING DETAIL



SIGN PANEL -

2 1/2" x 2 1/2" RUST-PROOF HEX. BOLT  
W/ 2 WASHERS AND 1 LOCK WASHER

SIGNS SHALL BE PLACED 4' BEHIND  
GUARDRAIL SO LONG AS THEY ARE NO  
CLOSER THAN 12' TO E NEAREST TRACK

### CASE 1

PLACE GUARDRAIL AS CLOSE  
TO CGR ROW AS POSSIBLE  
FINISH END (TYPE)---

FINCH LEP (TYP.)

CASE II

EXTEND GUARDRAIL BEYOND  
ROADWAY AT LEAST 2' (TYP.)

STIPULATION OF PARTIES  
FOR THE CLOSING OF CLYATT CIRCLE  
HIGHWAY-RAIL GRADE CROSSING  
NASSAU COUNTY, FLORIDA

Nassau County, hereinafter referred to as "COUNTY," CSX Transportation, Inc., hereinafter referred to as "RAILROAD," and Florida Department of Transportation, hereinafter referred to as "DEPARTMENT," by and through their undersigned representatives hereby file this Stipulation of

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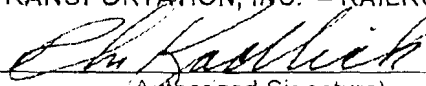
Parties as follows:

1. The COUNTY has filed an application with the DEPARTMENT to close the public highway-rail grade crossing at Clyatt Circle, Crossing Number 643314 X, pursuant to Section 335.141(1), Florida Statutes. A copy of the application is attached hereto and made a part hereof as EXHIBIT "A."
2. The Clyatt Circle crossing is a two (2) lane rural street railroad grade crossing located at RAILROAD Mile Post 617.48, near the Town of Hilliard, in Nassau County, Florida, as set forth on the map attached hereto and made a part hereof as EXHIBIT "B."
3. The railroad tracks at Clyatt Circle are classified as two (2) main line tracks and one (1) switch track. According to the RAILROAD, there are eighty-two (82) train movements per day. The current maximum train speed on the main line tracks at this location is seventy-nine (79) miles per hour.
4. The RAILROAD, at the RAILROAD's expense, will remove the crossing surface and roadway pavement ten (10) feet from both sides of the railroad track centerline at the Clyatt Circle crossing and restore the RAILROAD's right of way at the crossing with removal of all evidence of the crossing.

5. The RAILROAD, at the RAILROAD's expense, will remove the highway railroad grade crossing warning devices at the crossing. After closure work has been performed, the COUNTY, at the COUNTY's expense, will erect on each side of the closed crossings, if needed, signs and object markers as identified in the DEPARTMENT'S Standard Index 17349, outside of the RAILROAD'S right-of-way as provided in Exhibit "C."
6. All work by the COUNTY will be coordinated with the RAILROAD to ensure that all applicable railroad requirements such as flagging, insurance, etc. are adhered to.
7. ~~All parties agree that the closure of Clyatt Circle crossing can be applied, with the closure of the Harts Road highway-rail grade crossing (Crossing Number 620823 E), to the opening of a new crossing at the realignment of William Burgess Boulevard at a future date, if the COUNTY chooses to submit opening and closure applications, and provided the design plans are accepted by the RAILROAD and DEPARTMENT, and all work by the COUNTY is consistent with current Manual of Uniform Traffic Control Devices (MUTCD), American Association of State Highway and Transportation Officials (AASHTO) Policy, and the DEPARTMENT'S Manual of Uniform Minimum Standards For Design, Construction, and Maintenance for Streets and Highways (Florida's Green Book).~~
8. This Stipulation of Parties has been executed by all parties having an interest in this matter, and further, all parties of this stipulation waive hearing rights provided by Chapter 120, Florida Statutes, and request the DEPARTMENT to issue authority in accordance with Section 335.141(1), Florida Statutes and Rule 14-57.012, Florida Administrative Code.
9. The DEPARTMENT authorizes the closure of Clyatt Circle highway-rail grade crossing as evidenced by the execution of this Stipulation of Parties.

(THIS CONCLUDES THE BODY OF THIS STIPULATION OF PARTIES)

CSX TRANSPORTATION, INC. - RAILROAD

By:   
(Authorized Signature)

Printed Name: Richard M. Kadlick

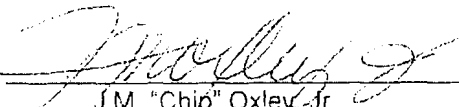
Title: Vice-President - Engineering & Mechanical Standards & Quality

Date: 1/7/04

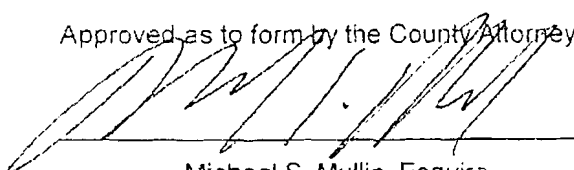
NASSAU COUNTY - COUNTY

Owner:  
Board of County Commissioner  
Nassau County, Florida

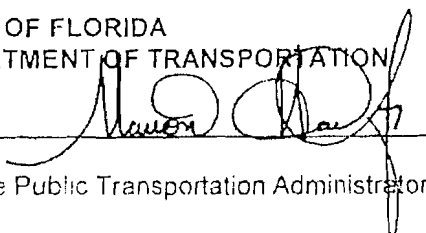
  
~~Floyd Vanzant~~ Chairman  
Vickie Samus

Attest:   
J.M. "Chip" Oxley, Jr.  
Its: Ex-Officio Clerk

Approved as to form by the County Attorney

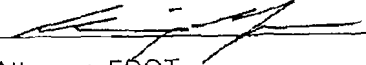
  
Michael S. Mullin, Esquire

STATE OF FLORIDA  
DEPARTMENT OF TRANSPORTATION

By:   
State Public Transportation Administrator

Date: 01-28-04

DEPARTMENT OF TRANSPORTATION  
LEGAL REVIEW

By:   
Attorney, FDOT

Date: 1-28-04

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EXHIBIT A of  
Clyatt Circle SOP

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION  
RAILROAD GRADE CROSSING APPLICATION

725-080-006  
RAC  
1000

ROAD NAME OR NUMBER	COUNTY/CITY NAME
CLYATT CIRCLE	NASSAU COUNTY

A. IDENTIFICATION:

Submitted By: \_\_\_\_\_ Application For: \_\_\_\_\_

Applicant JACK J. D'AMATO ☐ Closing a public at-grade rail/highway crossing by:

Title DIRECTOR OF PUBLIC WORKS ☒ roadway removal

Telephone 904-491-3610 ☐ Opening a public at-grade rail/highway crossing

Address 213 NASSAU PLACE by:

YULEE, FLORIDA 32097 ☐ new rail line construction

☐ new roadway construction

☐ Conversion of an existing crossing by:

☐ private crossing to public at-grade rail/highway crossing

B. CROSSING LOCATION:

FDOT/AAR Crossing Number: 643 314X

Jurisdiction for Street or Roadway by Authority of: ☐ City ☒ County ☐ State

Local Popular Name of Street or Roadway: CLYATT CIRCLE

Railroad Company: CSX

Railroad Mile Post: 617.48

SUBMITTED FOR THE APPLICANT BY: \_\_\_\_\_ DATE: 12-3-01

Title

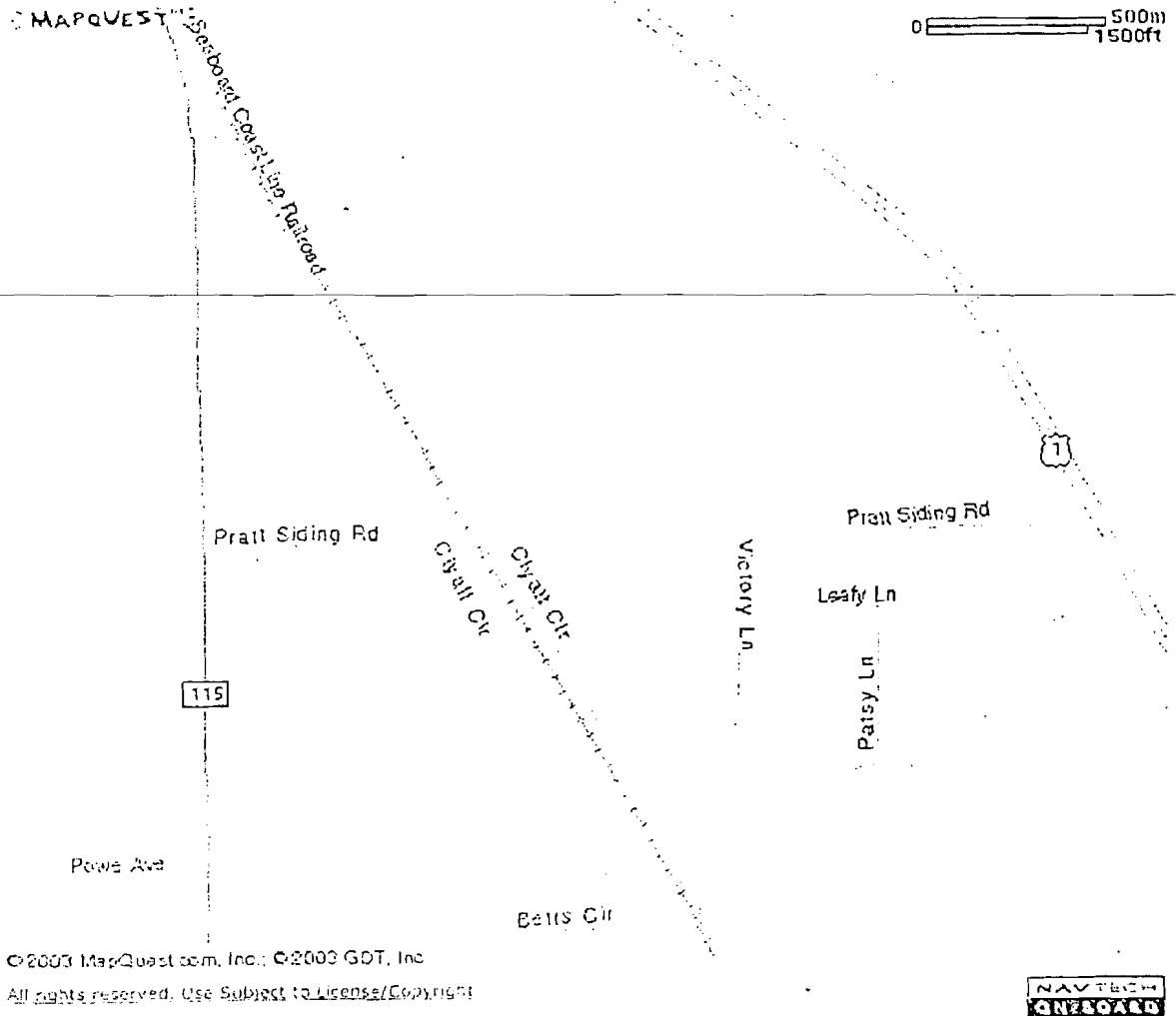
APPLICATION APPROVED FOR THE DEPARTMENT BY: \_\_\_\_\_ DATE: 12-3-01

Title - Railroad Corridor Program Engineer

REFERENCES:  
(Specific Legal Authority) 334.044 F.S., 120.57 F.S.  
(Law Implemented) 335.141 F.S.  
(Administrative Rule) 14-46 C03 D.O.T.  
Rail Manual Topic No. 725-080-002, Chapter 3

ATTACHMENTS:  
☐ Location Map  
☐ Letters of Support (opening/closing)

EXHIBIT B  
OF CLYATT CIRCLE SOT

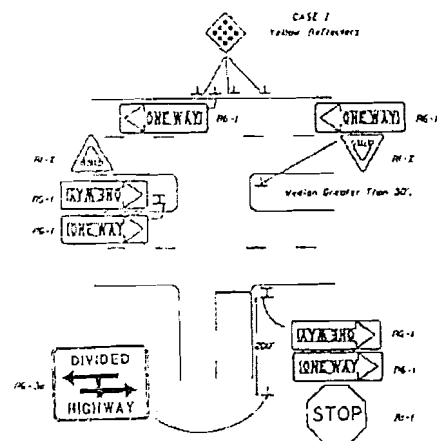


**CASE 1** Type 1 Collect Markers shall consist of nine yellow reflectors mounted on a yellow reflective background or consist of a reflective material the same size with Type 22-A, 22-B or 22-C yellow sheeting.

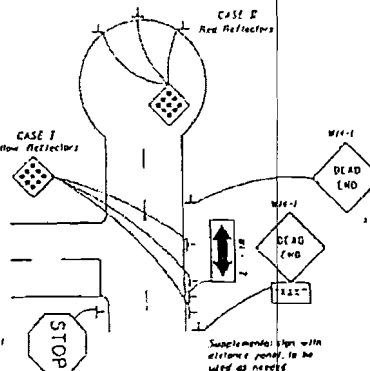
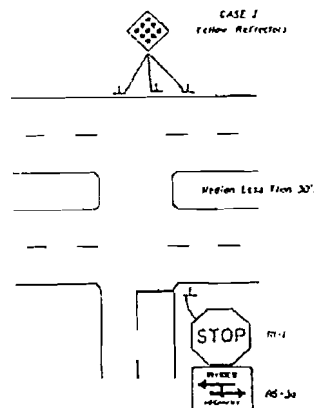
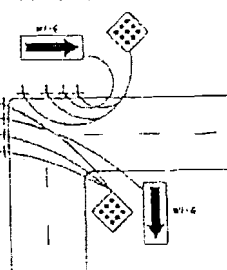
**CASE 2** One of three Markers shall consist of nine red reflectors mounted on a red reflective background or consist of a reflective material the same size with Type 22-A, 22-B or 22-C red sheeting.

#### NOTES:

1. This Index applicable to residential minor streets only. Major streets to be evaluated on a case by case basis.
2. "T"-Intersection (Two-way arrows and reflectors are natural). The head should be based on a review of each location.
3. For additional details on minimum round post, steel channel channel post, sign panel material and bolts, nuts and washers see Index No. 1063 and 1065.
4. Case 1 Installation - The arrow width and object markers shall be spaced approximately 20', but not less than 12' from the edge of the intersection.
5. One-way sign shall be posted a sufficient advance distance to permit the vehicle operator to make the dead-end by turning off, if possible, or the nearest intersecting street.
6. For government marking see Index no. 1036.
7. No general is required unless specifically conditions require its use.

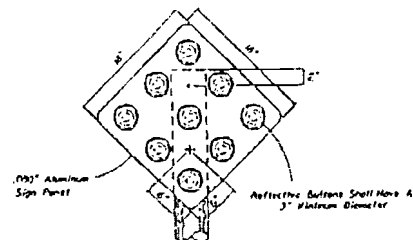
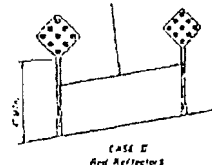


**CASE 1**  
Yellow Reflectors



**CASE 2**  
Red Reflectors

**CASE 3**  
Red Reflectors



Supports shall be driven 3' into the ground.

2" x 8" x 1/2" Aluminum Round Post or 2.5" x 1/2" Steel Flanged Channel Post.

Aluminum Post: 1/2" x 8" Aluminum Round Head Post with nut and lockwasher or 2" x 8" Stainless Steel Hex Head Bolt with Flat Washer under Head and Lockwasher under Nut.

Channel Posts Provide Attachment in Accordance with the "Sign Attachment Detail" on Index No. 1065.

ONE WAY signs (R6-1) are not ordinarily needed at divided highway intersections with median widths of less than 30', and should be installed only if specifically called for in the plans.

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION SAFETY DESIGN					
TRAFFIC CONTROLS FOR STREET TERMINATIONS					
DESIGNED BY	DATE	PROJECT NO.	SECTION	DATE	BY
DRIVEN BY	DATE	PROJECT NO.	SECTION	DATE	BY
CHECKED BY	DATE	PROJECT NO.	SECTION	DATE	BY



EXHIBIT 'G'

## U.S. DOT CROSSING INVENTORY FORM

B. Crossing Number (max 7 char.)		PAGE 2		D. Effective Date (MM/DD/YYYY)	
<b>Part III: Traffic Control Device Information</b>					
1. No Signs or Signals <input type="checkbox"/> Check if Correct		2. Type of Warning Device at Crossing - Signs (specify number of each)			
		2.A. Crossbucks	2.B. Highway Stop Signs (R1-1)	2.C. RR Advance Warning Signs (W10-1) <input type="checkbox"/> Yes <input type="checkbox"/> No	2.D. Mump Crossing Sign (W10-5) <input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> Unknown
2.E. Pavement Markings <input type="checkbox"/> Stripelines <input type="checkbox"/> RR Xing Symbols <input type="checkbox"/> None		2.F. Other Signs (specify MUTCD type) Number _____ Specify Type (max. 10 char.) _____ Number _____ Specify Type (max. 10 char.) _____			
3. Type of Warning Device at Crossing - Train Activated Devices (specify number of each)					
3.A. Gates	3.B. Four-quadrant (or full barrier) Gates <input type="checkbox"/> Yes <input type="checkbox"/> No	3.C. Cantilevered (or Bridged) Flashing Lights Over Traffic Lane (number) _____ Not Over Traffic Lane (number) _____		3.D. Mast Mounted Flashing Lights (number)	3.E. Number of Flashing Light Pairs
3.F. Other Flashing Lights: Number _____ Specify Type (max. 9 char.) _____		3.G. Highway Traffic Signals (number)		3.H. Wigwags (number)	3.I. Bells (number)
3.K. Other Train Activated Warning Devices (specify) (max. 9 char.) _____					
4. Specify Special Warning Device NOT Train Activated (max. 20 char.)			5. Channelization Devices With Gates <input type="checkbox"/> All Approaches <input type="checkbox"/> One Approach <input type="checkbox"/> None		
6. Train Detection <input type="checkbox"/> Constant Warning Time <input type="checkbox"/> DC/AFD <input type="checkbox"/> Motion Detectors <input type="checkbox"/> Other		7. Signalling for Train Operation Is Track Equipped with Train Signals? <input type="checkbox"/> Yes <input type="checkbox"/> No		8. Traffic Light Interconnection/Preemption <input type="checkbox"/> Not Interconnected <input type="checkbox"/> N/A <input type="checkbox"/> Simultaneous Preemption <input type="checkbox"/> Advance Preemption	
9. Reserved For Future Use		10. Reserved For Future Use		11. Reserved For Future Use	
12. Reserved For Future Use					
<b>Part IV: Physical Characteristics</b>					
1. Type of Development <input type="checkbox"/> Open Space <input type="checkbox"/> Residential <input type="checkbox"/> Commercial <input type="checkbox"/> Industrial <input type="checkbox"/> Institutional			2. Smallest Crossing Angle <input type="checkbox"/> 0° - 29° <input type="checkbox"/> 30° - 59° <input type="checkbox"/> 60° - 90°		
3. Number of Traffic Lanes Crossing Railroad		4. Arc Truck Pullout Lanes Present? <input type="checkbox"/> Yes <input type="checkbox"/> No		5. Is Highway Paved? <input type="checkbox"/> Yes <input type="checkbox"/> No	
6. Crossing Surface (on main line) <input type="checkbox"/> 1. Timber <input type="checkbox"/> 2. Asphalt <input type="checkbox"/> 3. Asphalt and Flange <input type="checkbox"/> 4. Concrete <input type="checkbox"/> 5. Concrete and Rubber <input type="checkbox"/> 6. Rubber <input type="checkbox"/> 7. Metal <input type="checkbox"/> 8. Unconsolidated <input type="checkbox"/> 9. Other (Specify) _____					
7. Does Track Run Down a Street? <input type="checkbox"/> Yes <input type="checkbox"/> No		8. Nearby Intersecting Highway? <input type="checkbox"/> Less than 75 feet <input type="checkbox"/> 75 to 250 feet <input type="checkbox"/> 250 to 500 feet <input type="checkbox"/> N/A		Is it Signalized? <input type="checkbox"/> Yes <input type="checkbox"/> No	
9. Is Crossing Illuminated? (street lights within approx. 30 feet from nearest rail) <input type="checkbox"/> Yes <input type="checkbox"/> No		10. Is Commercial Power Available? <input type="checkbox"/> Yes <input type="checkbox"/> No		11. Space Reserved For Future Use	
<b>Part V: Highway Information</b>					
1. Highway System <input type="checkbox"/> Interstate <input type="checkbox"/> Federal Aid Not NHS <input type="checkbox"/> Nat Hwy System (NHS) <input type="checkbox"/> Non Federal Aid		2. Is Crossing on State Highway System? <input type="checkbox"/> Yes <input type="checkbox"/> No		3. Functional Classification of Road at Crossing	
5. Annual Average Daily Traffic (AADT) Year _____ AADT _____		6. Estimate Percent Trucks		7. Average Number of School Buses Over Crossing per School Day	

Paperwork Reduction Act: Public reporting for this information collection is estimated to average 15 minutes per response, including the time for reviewing instructions, searching existing data sources, gathering and maintaining the data needed, and completing and reviewing the collection of information. According to the Paperwork Reduction Act of 1995, no persons are required to respond to a collection of information unless it displays a currently valid OMB Control Number. The valid OMB Control number for this collection is 2120-0017.

# U.S. DOT CROSSING INVENTORY FORM

DEPARTMENT OF TRANSPORTATION  
FEDERAL RAILROAD ADMINISTRATION (FRA)

OMB Control No. 2130-0017  
Expires: 3/31/2023

A. Initiating Agency <input type="checkbox"/> Railroad <input type="checkbox"/> State	B. Crossing Number (max. 7 char.)	C. Reason for Update <input type="checkbox"/> Changes in Existing Data <input type="checkbox"/> New Crossing <input type="checkbox"/> Closed Crossing or Abandoned	D. Effective Date (MM/DD/YYYY)
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## Part I: Location and Classification Information

1. Railroad Oper. Co. (code (max. 4 char.) or name)		2. State (2 char.)	3. County (max. 20 char.)
4. Railroad Division or Region (max. 14 char.)	5. Railroad Subdivision or District (max. 14 char.)	6. Branch or Line Name (max. 15 char.)	7. RR Milepost (max. 7 char.) (nnnnn.n)
8. RR I.D. No. (max. 10 char.)	9. Nearest RR Timetable Station (max. 15 char.) (optional)	10. Parent RR (max. 4 char.) (if applicable)	11. Crossing Owner (RR or Company name) (if applicable)
12. City (max. 16 char.) (check one) <input type="checkbox"/> In <input type="checkbox"/> Near		13. Street or Road Name (max. 17 char.)	STATE SUPPLIED INFORMATION
14. Highway Type & No. (max. 7 char.)		15. ENS Sign Installed (1-200) <input type="checkbox"/> Yes <input type="checkbox"/> No	21. HSR Corridor ID (2 char.)
16. Quiet Zone <input type="checkbox"/> No <input type="checkbox"/> 24 hr <input type="checkbox"/> Partial <input type="checkbox"/> Unknown		22. County Map Ref. No. (max. 10 char.)	
17. Crossing Type (choose one only) <input type="checkbox"/> Public <input type="checkbox"/> Private <input type="checkbox"/> Pedestrian	18. Crossing Position <input type="checkbox"/> At Grade <input type="checkbox"/> RR Under <input type="checkbox"/> RR Over	19. Type of Passenger Service <input type="checkbox"/> AMTRAK <input type="checkbox"/> AMTRAK & Other <input type="checkbox"/> Other <input type="checkbox"/> None	20. Average Passenger Train Count Per Day
23. Latitude (max. 10 char., nn.nnnnnn)		24. Longitude (max. 11 char., nnn.nnnnnn)	
25. Lat/Long Source <input type="checkbox"/> Actual <input type="checkbox"/> Estimated			
26. Is There an Adjacent Crossing With a Separate Number? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes, Provide Number _____ (7 characters)			

## 27. PRIVATE CROSSING INFORMATION

27.A. Category (check one) <input type="checkbox"/> Recreational <input type="checkbox"/> Farm <input type="checkbox"/> Residential <input type="checkbox"/> Industrial <input type="checkbox"/> Commercial	27.B. Public Access <input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> Unknown	27.C. Signs/Signals <input type="checkbox"/> None <input type="checkbox"/> Signs Specify (max. 15 char.) _____ <input type="checkbox"/> Signals Specify (max. 15 char.) _____
28.A. Railroad Use (max. 20 char.)	29.A. State Use (max. 20 char.)	
28.B. Railroad Use (max. 20 char.)	29.B. State Use (max. 20 char.)	
28.C. Railroad Use (max. 20 char.)	29.C. State Use (max. 20 char.)	
28.D. Railroad Use (max. 20 char.)	29.D. State Use (max. 20 char.)	

30. Narrative (max. 100 char.)

31. Emergency Contact (Telephone No.)	32. Railroad Contact (Telephone No.)	33. State Contact (Telephone No.)
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## MUST COMPLETE REMAINDER OF FORM FOR PUBLIC VEHICLE CROSSINGS AT GRADE

### Part II: Railroad Information

1. Number of Daily Train Movements			
1.A. Total Trains	1.B. Total Switching Trains	1.C. Total Daylight Thru Trains (6 AM to 6 PM)	1.D. Check if Less Than One Movement Per Day <input type="checkbox"/>
2. Speed of Train at Crossing			
2.A. Maximum Time Table Speed (mph) _____			
2.B. Typical Speed Range Over Crossing (mph) from _____ to _____			
3. Type and Number of Tracks			
Main _____ Other _____ If Other, Specify (max. 10 char.) _____			
4. Does Another RR Operate a Separate Track at Crossing? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes, Specify RR (max. 16 char.) _____		5. Does Another RR Operate Over Your Track at Crossing? <input type="checkbox"/> Yes <input type="checkbox"/> No If Yes, Specify RR (max. 16 char.) _____	

## Brenda Linville

---

**From:** Bordelon, Janice L [Janice.Bordelon@dot.state.fl.us]  
**Sent:** Monday, November 30, 2009 1:42 PM  
**To:** Brenda Linville  
**Subject:** RE: Stipulation of Parties to Open William Burgess Blvd. Highway-Rail Grade Crossing and Close Harts Road Highway-Rail Grade Crossing

Brenda: We are no longer mailing hard copies of the Stipulation of Parties. The executed Stipulation was electronically sent and received by Nassau County as verified below.

---

**From:** jpage@nassaucountyfl.com [mailto:jpage@nassaucountyfl.com]  
**Sent:** Wednesday, October 21, 2009 11:50 AM  
**To:** Bordelon, Janice L  
**Subject:** RECEIVED: Attachment Executed Stipulation of Parties Wm Burgess.pdf

### Return Receipt

Your attachment: Executed Stipulation of Parties Wm Burgess.pdf Was downloaded at: 21 October 2009 11:49:46 By recipient: jpage@nassaucountyfl.com File size is: 11.00 MB

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Please let me know if you need for me to add you to the mailing list. Thank you.

Jan Bordelon, Rail Office  
Florida Department of Transportation  
605 Suwannee Street, MS 25  
Tallahassee, FL 32399-0450  
850-414-4528

Never race a train to the crossing - even if you tie, you lose!